

Department of Planning and Community Development Documents Library – Document Cover Sheet

Title of Document: East Beach Harbour Concept Studies

Prepared By: Urban Design Associates

Prepared For: City of Norfolk and NRHA

Date of Preparation: March 2004

Status (as of January 2012): The General Plan was amended on March 28, 2006 to make the East Beach Harbour area Mixed Use on the Future Land Use Map. The Zoning Ordinance was also amended at this time to create a special purpose zoning district that incorporated the recommendations of this plan. Some plan actions reaffirmed by City Council in 2009.

Civic League(s)/Organization(s) Affected: East Ocean View, East Beach

The information included in this document may not reflect current City of Norfolk policy. For more information on this document, contact the City of Norfolk Department of Planning and Community Development.

DRAFT 27 JANUARY 2006

EAST BEACH HARBOR MIXED-USE WATERFRONT DISTRICT
DEVELOPMENT GUIDELINES

Norfolk, Virginia

URBAN DESIGN ASSOCIATES

JANUARY 2006

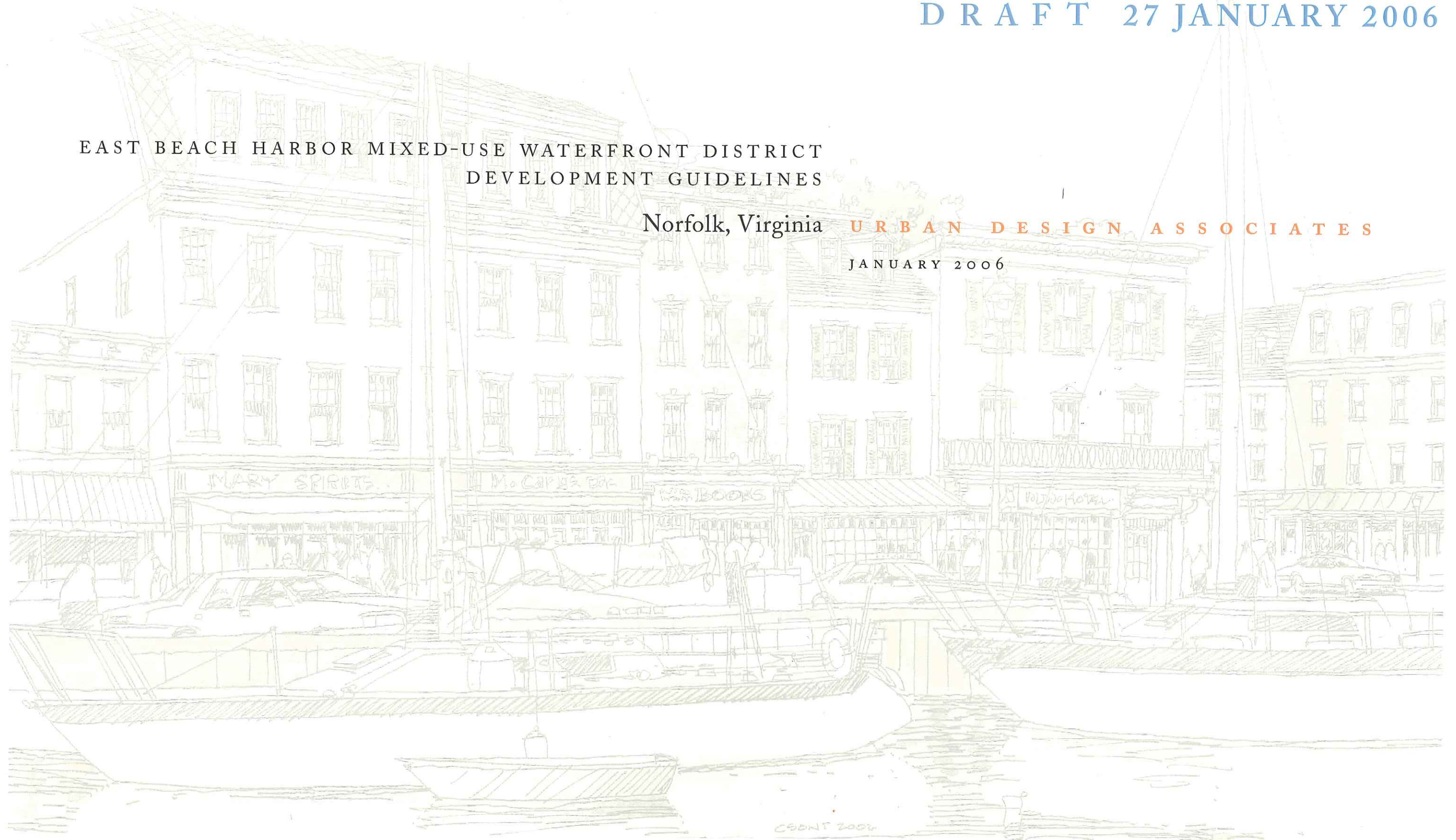


Table of Contents

INTRODUCTION 1

DESIGN PRINCIPLES 2

- Design the Land Use Strategy
- Complement East Beach Development
- Build the Architectural Traditions of the Waterfront
- Strengthen Public Spaces

DESIGN STRATEGIES 3

GENERAL DEVELOPMENT STANDARDS 4

- Property Ownership 5
- Regulating Plan, Option A 6
- Open Space Plan 7
- Building Heights Plan 8
- Parking Zones Plan 9
- Regulating Plan, Preferred Option 10

GENERAL DESIGN GUIDELINES 11

- Inventory of Building Types 12
- Mixed Use Buildings 13
- Townhouse Buildings 14
- Wharf Buildings 15

DISTRICT CHARACTER AREAS 16

- East Beach Character Area 17
- Pretty Lake Drive Character Area 20
- Harbor Character Area 22

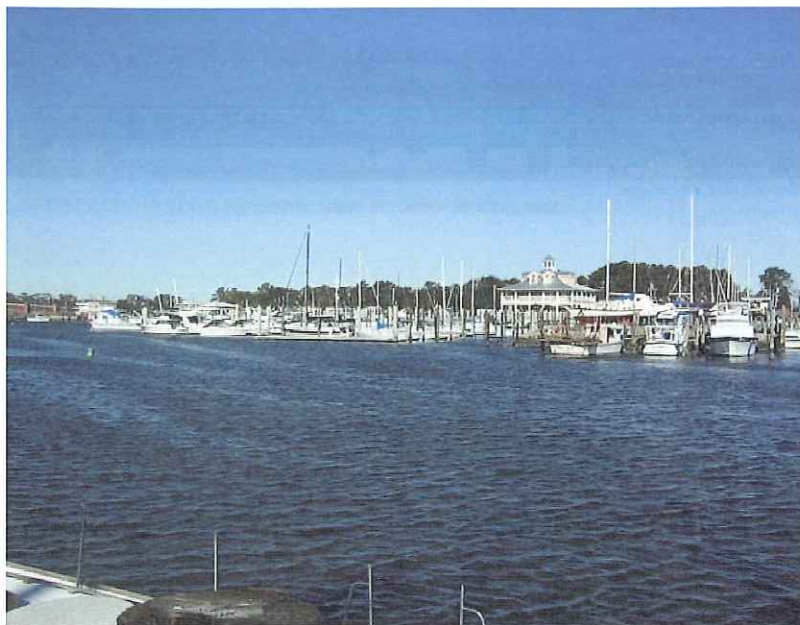
DEVELOPMENT APPROVAL PROCESS 24

Introduction

THE EAST BEACH HARBOR AREA aligns the northern edge of Little Creek Channel at the southern point of the Chesapeake Bay. The future of the waterfront is important to the long term improvement of adjacent neighborhoods. While much of the river frontage today is a working waterfront, land uses are changing and opportunities to share the waterfront are emerging.

Many coastal towns such as Edenton, North Carolina and Annapolis, Maryland have waterfront districts with a mix of residential and commercial uses that contrast in character with the inland residential neighborhoods. East Beach Harbor will have a continuous series of public spaces and thoroughfares that provide access to the waterfront for residents, boat slip users and visitors.

This document is intended to provide development regulations and design guidelines for developers, design professionals and authorities of the East Beach Harbor Mixed Use Development District. It defines the elements important to the design of public spaces and buildings within the District. The *Development Regulations* describe required elements, such as building zones, setbacks, and building heights at the scale of both the district and the individual block. The *Design Principles* illustrate the vision for buildings and streetscapes. The *District Addresses* provide more detailed information for buildings and the principles that should guide their design.

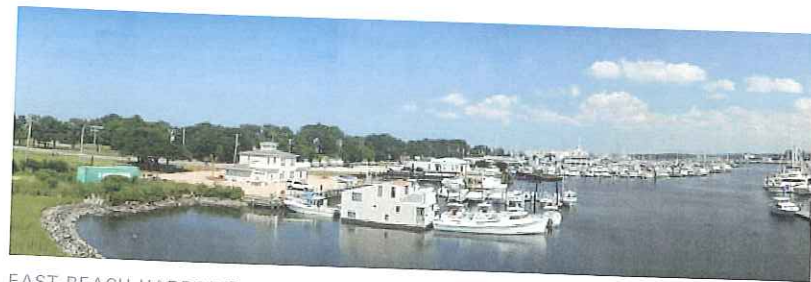


EAST BEACH HARBOR View of the Harbor from across Little Creek Channel.



AERIAL VIEW East Beach and East Beach Harbor looking south.

Design Principles



EAST BEACH HARBOR Panoramic view of harbor looking east



EAST BEACH HARBOR View of harbor looking east

The marinas along Pretty Lake Avenue create a backdrop for a potential mixed-use waterfront district with a mix of shops, offices and residential units with a direct connection to the marinas along the shore.

To achieve this goal, the consultant team, working with the City of Norfolk Planning Department, the Norfolk Redevelopment and Housing Authority and the property owners committee, created urban design and development principles to guide development in the East Beach Harbor Mixed-Use Waterfront District.

PRINCIPLE 1 Define a New Land Use Strategy

East Beach Harbor is currently zoned as I-4 Waterfront Industrial District. It is used for water-dependent activities in which outdoor storage is permitted. The land use strategy should cultivate a mixed-use development, creating a special place to live and visit.

PRINCIPLE 2 Complement East Beach Development

The future of East Beach Harbor is tied to a revitalized waterfront, one of the area's greatest assets. Currently, the pattern of predominantly lower density industrial and warehouse uses along the waterfront fails to capitalize on its full potential of this prime waterfront location. The creation of a new mixed-use waterfront development district will provide amenities and activities for the residential developments in the area.

PRINCIPLE 3 Build Upon the Architectural Traditions of the Waterfront.

East Beach Harbor will build upon the character of traditional Tidewater waterfront architecture as well as the character of East Beach itself.

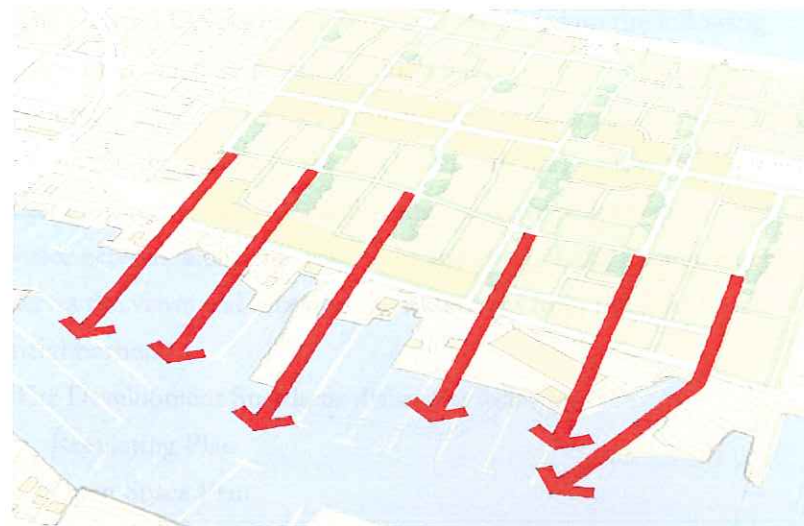
PRINCIPLE 4 Enhance the Character of Public Spaces

Public spaces along waterfronts can support mixed-use districts with a diverse range of spaces and addresses.

View corridor easements to the south of Pretty Lake Avenue, from 24th Bay through 29th Bay, and waterfront pedestrian sidewalks have been established to preserve the public connection to the waterfront.

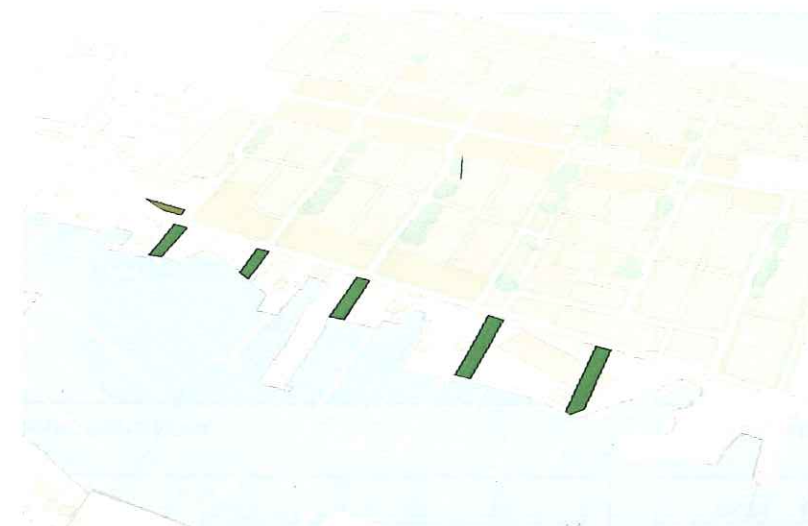
The public rights-of-ways need to be sufficient to accommodate pedestrians, landscaping and an appropriately sized cartway; allowing service and emergency vehicles access to the mixed-use East Beach Harbor development and the Bayfront Development to the east.

Design Strategies



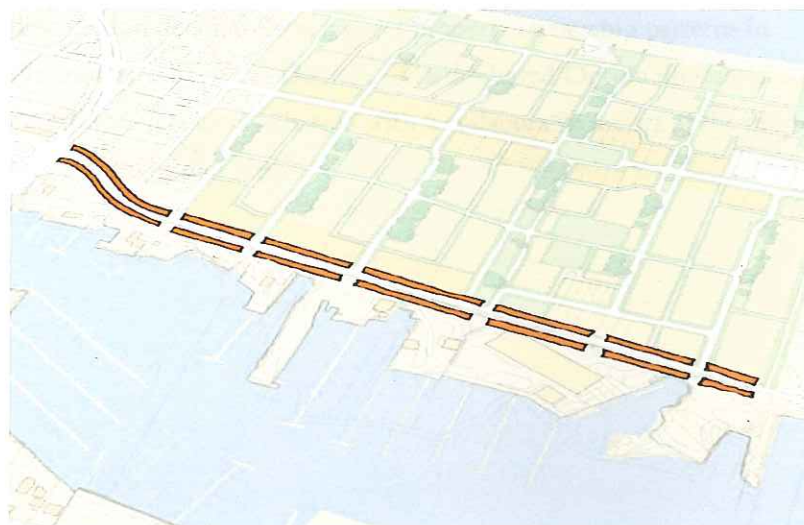
VIEW CORRIDORS

The north-south streets which end at Pretty Lake Avenue should provide open vistas to the harbor in order provide clear views for the residential neighborhood to the north.



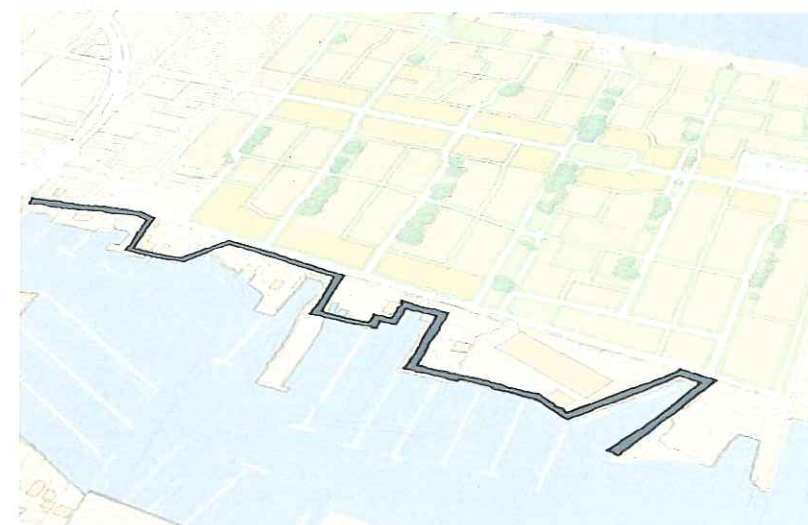
OPEN SPACE

The alignment of the north-south streets will be extended onto the waterfront properties in the form of view easements. No development will be permitted to block these views. The public open space that is created should be designed as primarily pedestrian areas and can serve as the main address of the development on either side.



STREET SPACE

Pretty Lake Avenue will be the main street for this area. It will have ample sidewalks and will be lined with active uses.



PEDESTRIAN LINKAGE ALONG WATERFRONT

The ideal form would be a proximate public walkway along the waterfront.

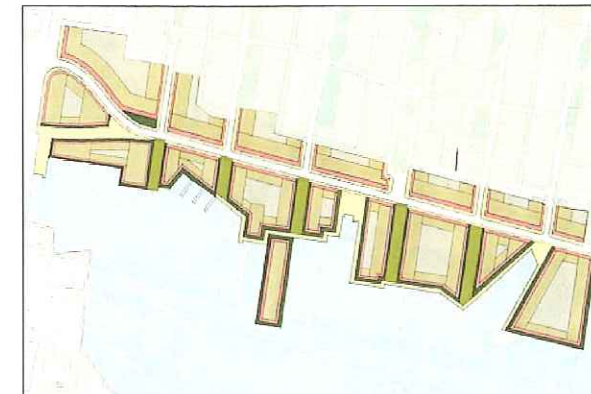
General Development Standards

The General Development Standards contained on the following pages offer parameters within which the development and design plan must adhere. The Development Standards are based on locating parking in the middle of the block and surrounding it with buildings that have active uses. This will define an active pedestrian and open space network along the waterfront and Pretty Lake Avenue and preserves the views and access to the waterfront from the East Beach neighborhood.

The Development Standards define the following:

- Regulating Plan
- Open Space Plan
- Building Heights Plan
- Parking Zones Plan

There are two alternative regulating plans. Option A which is described in detail, follows current property ownership patterns in the western part of the District. The Preferred Option illustrates a design which could be achieved with a collaborative effort of the property owners in that area.



REGULATING PLAN



OPEN SPACE PLAN



BUILDING HEIGHT PLAN



PARKING ZONES PLAN

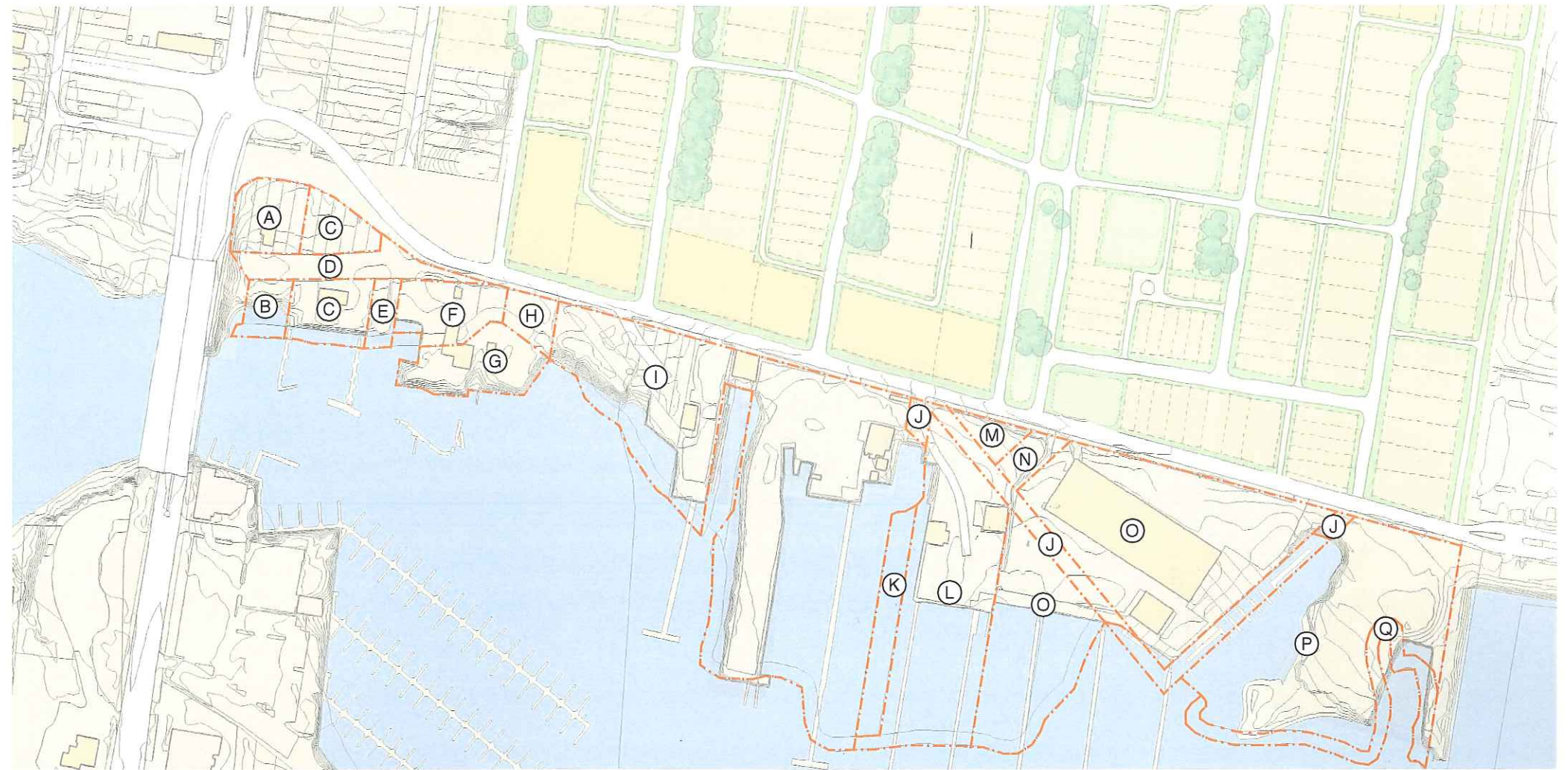
Property Ownership

The complex configuration of property ownerships presents a series of challenges to the development of a coordinated plan. There are many small properties in some cases by former rights of way which are now privately owned. This is most critical in the western portion of the district in which the resultant parcels make it difficult to develop a coordinated parking configuration.

Therefore, there are two design options presented in these guidelines:

Option A: With current property configurations

Preferred Option: With combined properties



OWNERSHIP PLAN Illustrates the multiple and fragmented ownership patterns within the East Beach Harbor Mixed-Use waterfront District

- A. EAST BEACH EXCHANGE LLC
- B. POYNER ROAD, LLC
- C. EDWARDS, LISA YEISER
- D. EDWARDS, LISA(FEE IN STREET)
- E. EAST BEACH.
- F. JACOBS, VIRGINIA.
- G. N/A.
- H. GARCIA, DOMINICK.
- I. ESPEJO FAMILY LIMITED PTNSHP.
- J. CLARK, ED
- K. GARRETT, WILLIAM
- L. CAMPBELL
- M. MAGGARD, MILDRED
- N. DENNY, ELTON
- O. LITTLE CREEK PARTNERS LTD PTNSHP
- P. M EASTBEACH LLC
- Q. VINNINGS MARINE GROUP

Regulating Plan: Option A

The Regulating Plan sets the dimensions of the streets as indicated in the section drawings, development blocks, building zones, front yards, and open space for every block. The following terminology is used in the regulations, and definitions have been developed for use in this Block Plan:

Building Zone Area within a block in which buildings may be placed.

Facade Zone The area between the Front Yard and the Building Zone within which the front facade of the building must be located. The front edge of the Facade Zone describes the extent of the building envelope. Porches, bay windows, stoops, and stairs must be located in the Facade Zone.

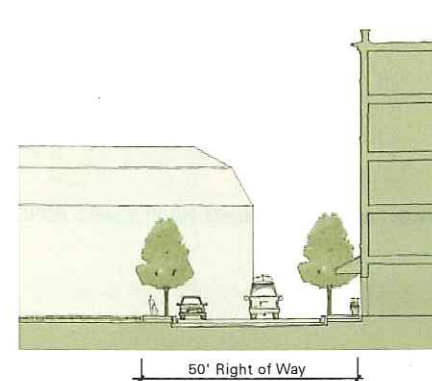
Front Yard The area between the property line and the Facade Zone.

Parking Zones Parking zones are designated areas where parking is permitted. Parking areas shall be screened.

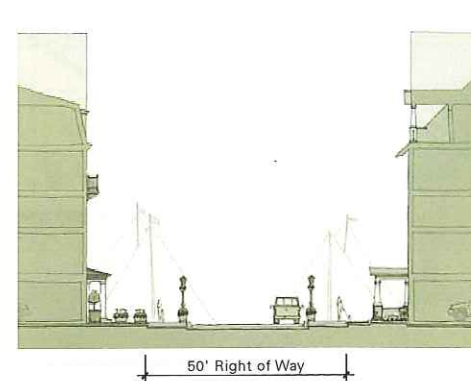
View Corridor Easement Public access easements, where the Bay streets terminate at Pretty Lake Avenue, are provided to the south of Pretty Lake Avenue to maintain harbor views, provide public access to the open space network along the waterfront and to provide a limited access drive aisle for interior block parking. They may also serve as addresses for development.



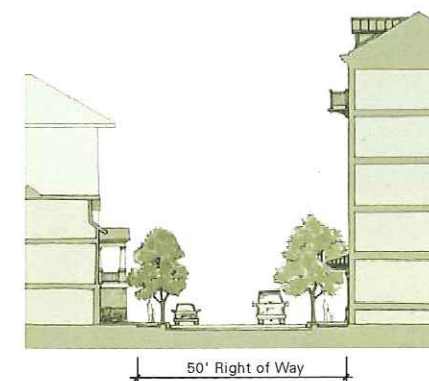
REGULATING PLAN Prescribes the dimensions of all streets, blocks and open space to create a coherent development plan.



A LITTLE ANNAPOLIS ADDRESS SECTION



B HARBOR ADDRESS SECTION



C PRETTY LAKE AVENUE ADDRESS SECTION

- BUILDING ZONES
- FACADE ZONES
- FRONT YARDS
- PARKING ZONES
- VIEW CORRIDOR EASEMENT
- WALKWAY

Open Space Plan: Option A

There will be a continuous network of pedestrian spaces along and to the waterfront. There will be six types of space as indicated in the diagram:

Verge The sidewalks will have either trees in wells or a continuous verge with ornamental grasses and trees.

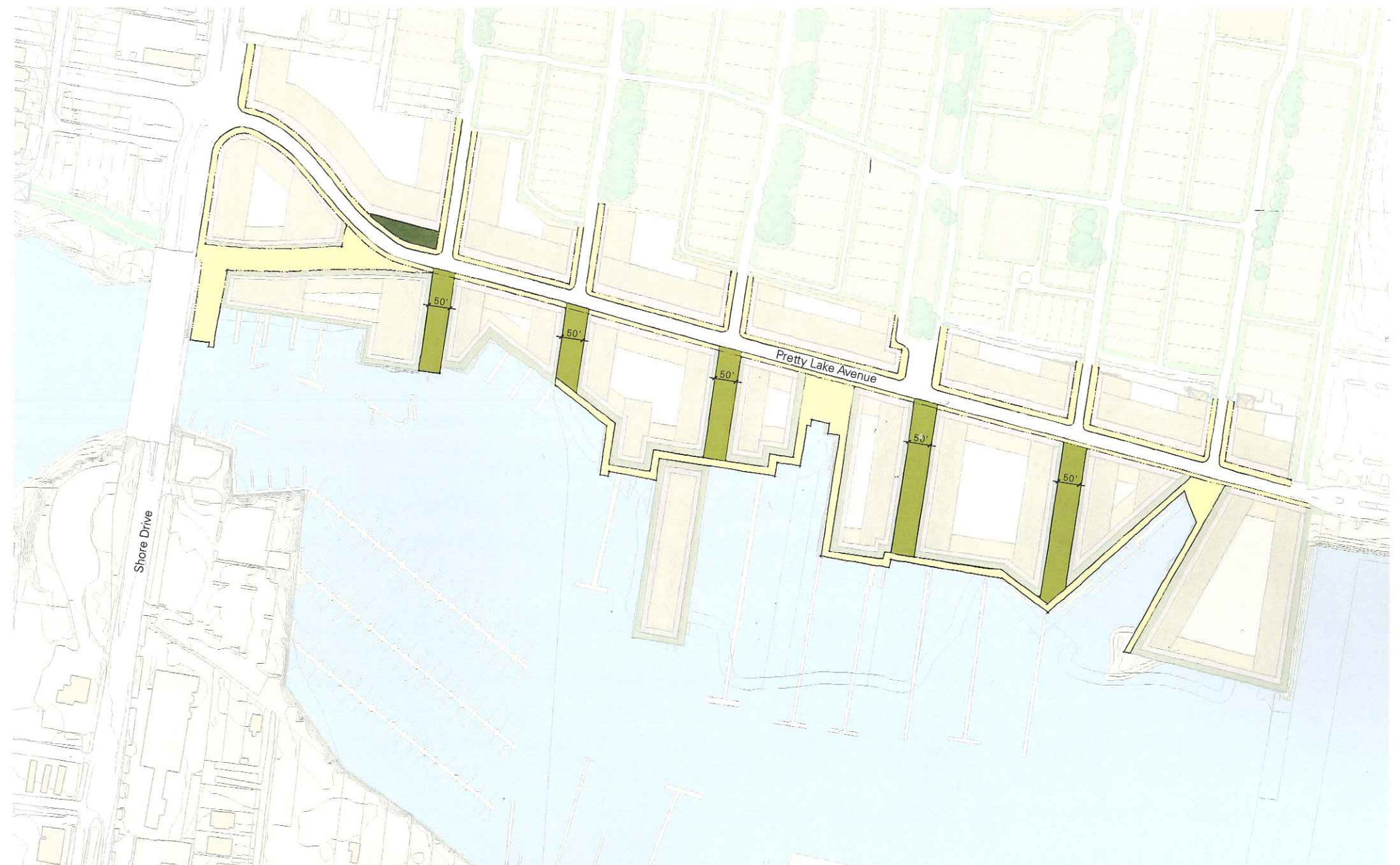
Parks and Open Space A continuous series of public spaces and thoroughfares provide access to the waterfront for residents, visitors, and boaters. A combination of waterfront promenade, plazas and greens provide a variety of experiences.

Streets The neighborhood Bay streets of East Beach offer rich visual connections to the Little Creek Channel. Where the Bay streets terminate at Pretty Lake Avenue, public access easements are provided on the south side to maintain harbor views.

Sidewalks The streets will be lined with ample sidewalks with delineation of pedestrian crossings at intersections. The major east west public space will be Pretty Lake Avenue with sidewalks on both sides of the streets. The north-south streets will end at Pretty Lake but should have cross walks designed as extensions of the sidewalks on each side of the streets.

View Corridor Easement Public access easements are provided to the south of Pretty Lake Avenue to extend alignment of the East Beach streets in order to maintain harbor views, provide public access to the open space network along the waterfront and to provide a limited access drive aisle for interior block parking. They may also serve as addresses for development.

Waterfront Walk A continuous public open space system along or near the waterfront.

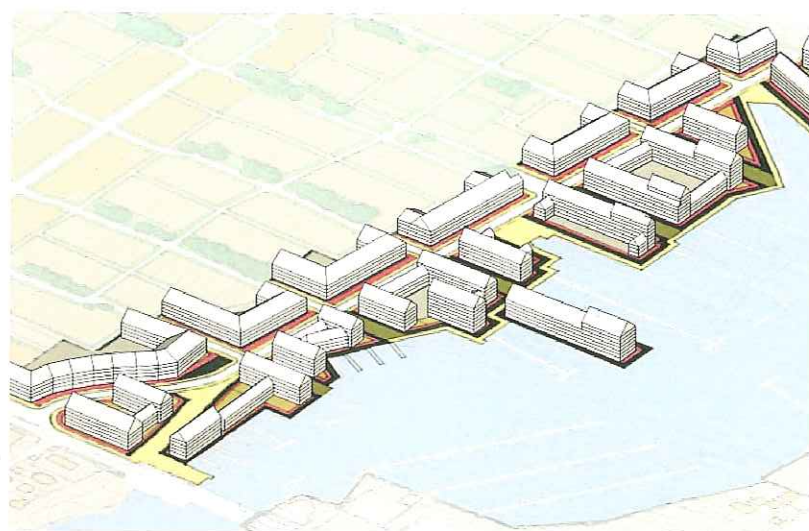


OPEN SPACE PLAN Displays parks, open space and view corridors to the Harbor

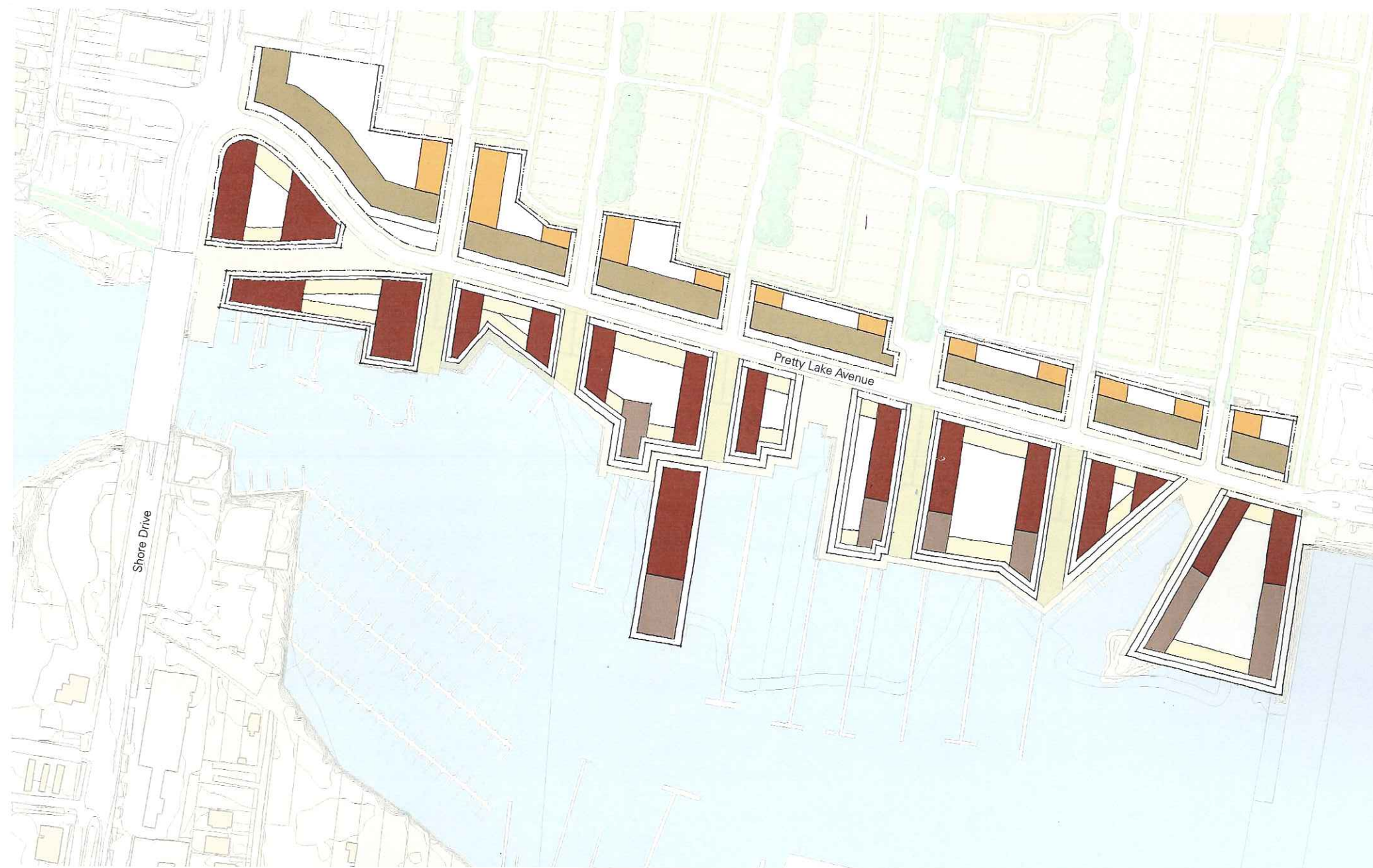
OPEN SPACE
WALKWAY / SIDEWALK
VIEW CORRIDOR EASEMENT

Building Heights: Option A

Building heights are regulated to help control the form and character of public spaces. The scale of an East Beach neighborhoods to the north of Pretty Lake Avenue is extended into the East Beach Harbor Mixed-Use Waterfront District along the north-south Bay streets terminating at Pretty Lake Avenue. The heights are proportional to the street widths and form intimate, pedestrian-friendly streets and views which are in character with the attached unit buildings of East Beach. Taller buildings are located nearest the water providing extraordinary views out over Little Creek Channel.



A diagram of maximum building heights within the district



BUILDING HEIGHTS A variety of building scales strengthen waterfront views and complements East Beach



Parking Strategy Plan: Option A

Parking Zones Parking must be located in the interior of development blocks and be screened from public view. There shall be a minimum of 10'-0" depth of active uses along public spaces. This includes Pretty Lake Avenue, the view easement spaces and along waterfront walks. Parking in some cases will need to be on the ground floor of multi-story buildings. Access into interior of blocks may be provided along the view corridor easements.



PARKING STRATEGY A coordinated design of parking resulting in a clear and coherent block structure

PARKING ZONES 
PARKING AND LOADING ACCESS 

Building Heights: Option A

By eliminating the diagonal East-West public right of way across the western waterfront properties, it is possible to create a larger development parcel. This makes it possible to embed parking in the middle of the block and to create higher density development.

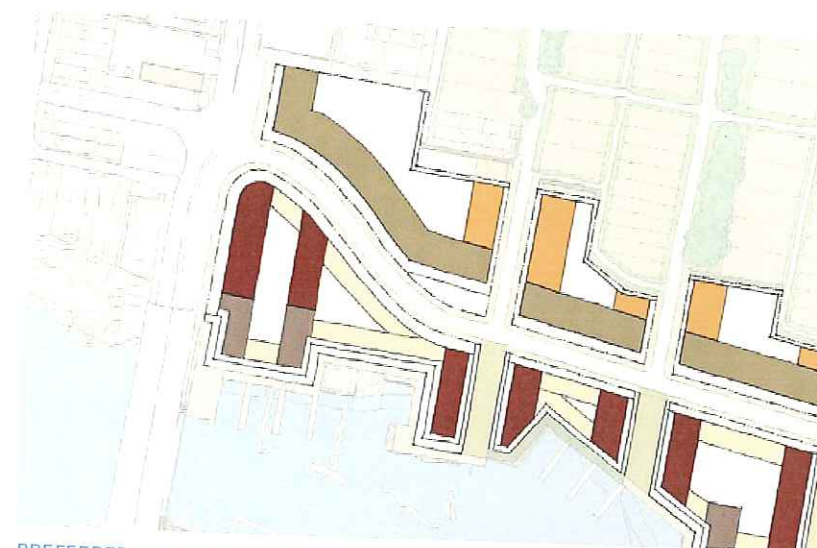
The principles and the regulations follow the same principles as Option A.



PREFERRED REGULATING PLAN



PREFERRED OPEN SPACE PLAN



PREFERRED BUILDING HEIGHTS PLAN



PREFERRED PARKING STRATEGY PLAN

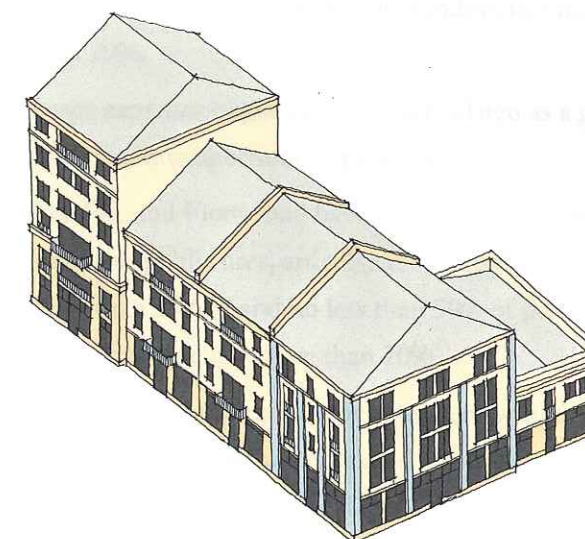
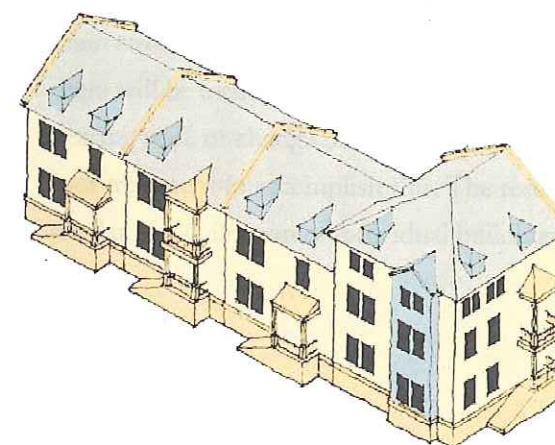
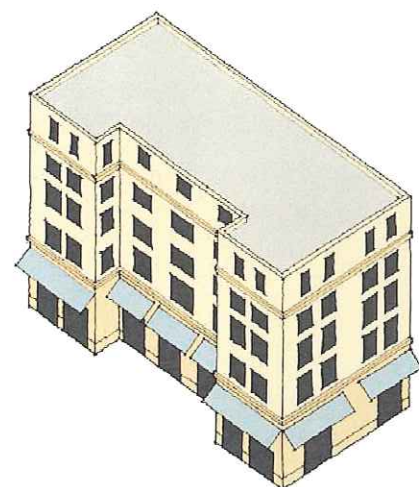
DRAFT

General Design Guidelines

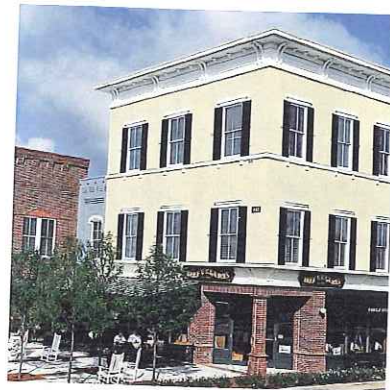
© 2005 URBAN DESIGN ASSOCIATES

Inventory of Building Types

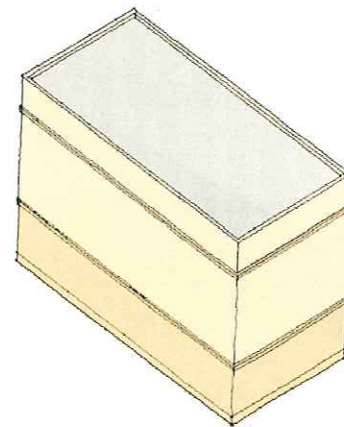
THE PLAN IDENTIFIES three building types that are recommended for the perimeter of these blocks. For each type, the primary goal of the architectural design should be to create beautiful public spaces, each of which will become a unique address within the city and the region. Therefore, the design guidelines define the exterior of the building blocks. The developers and their architects will determine the most efficient internal plans and arrangements within the block. The guidelines establish criteria for the exteriors. The building types are described in three dimensional views. They can be placed as indicated in the regulating plan. On the following pages, specific architectural guidelines are defined for each of these building types.



Mixed-Use Building (3–5 Stories)

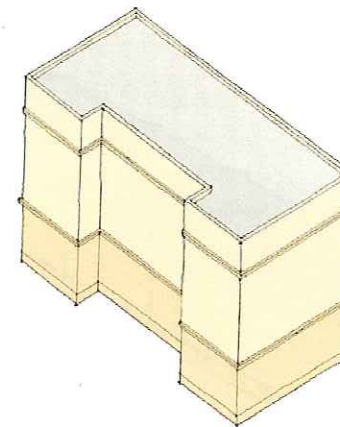


Building zone



- The buildings will be 3 to 5 stories in height.
- The first floor shall be taller than the upper floors.
- The top of the building may have a sloped roof which may contain a loft or additional usable space.

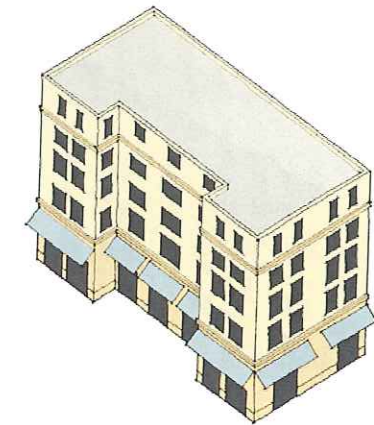
Building envelope



- The first floor of building's facade will have a cornice line or other element which defines a 'base' for the building.
- Top floor will have either a course line or a change of material or window pattern to define a 'top.'
- The long building form will be articulated so that it resembles a collection of party wall buildings on a main street. Therefore, at intervals of 30 to 50 feet there will be a change of plane, or extension of a bearing wall, or change of material, and a change of roof treatment to accomplish this. The result will be similar to a collection of individual buildings.

DRAFT

Building articulation

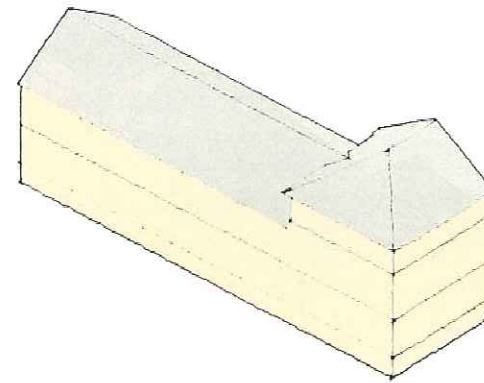


- Special elements include box and bay windows, two-story windows, recessed balconies, and roof elements.
- Windows shall be vertically-proportioned on the upper floors in which the lineal frontage of each floor will be no less than 40% in window, nor more than 70%.
- Large expanses of glass must be articulated as a group of vertically proportioned openings.
- The Ground Floor shall have shopfront windows for retail and public uses, articulated into bays of no more than 15 feet and no less than 50% of the lineal facade frontage nor more than 70%.
- Fenestration patterns shall be consistent within each 'individual building' as articulated on the facade, but vary from 'building' to 'building.'

Townhouse Building (2–3 Stories)

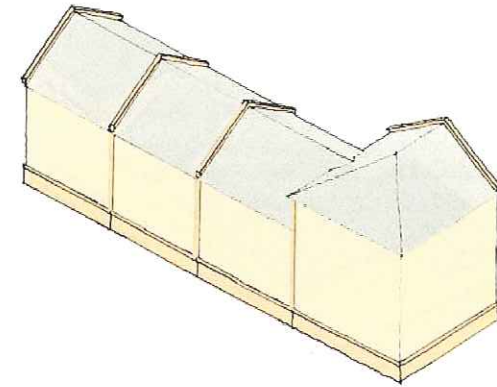


Building zone



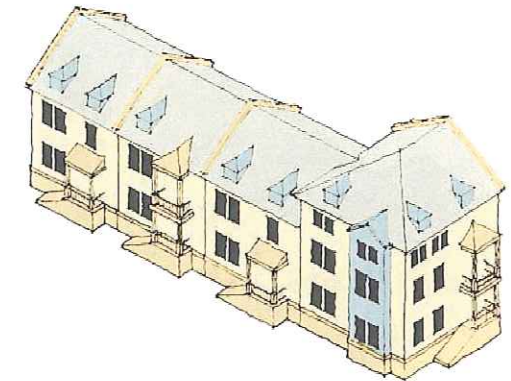
- The massing of the Townhouse building type shall be between 3 and 4 stories and have sloped roofs.
- Roof dormers are encouraged.

Building envelope



- The 3- to 4-story buildings should be articulated to be similar to a pattern of individual buildings.
- Articulation accomplished with physical breaks in the facade, be recessing small lengths of the facade (minimum 3'-0" x 2'-0" deep), by changing the plane of the facade (no more than 2'-0") or by projecting bearing walls between bays of the buildings.
- These changes should occur in intervals of no less than 20 feet and no more than 40 feet. The result will be a collection of individual facades.

Building articulation

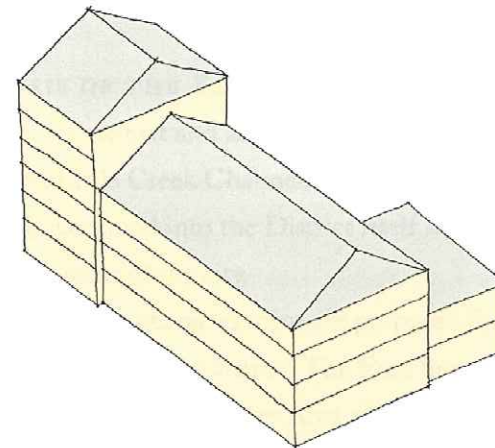


- The buildings in East Beach have a rich variety of porches, bay windows, and roof elements. Each individual facade should have at least one such element.
- Porches and other extensions into the front yard space may not exceed 35% of the lineal frontage of the facade.
- Windows should be of vertical, classically-proportioned openings.
- Windows shall occupy a minimum of 45% of the lineal frontage of each facade and a maximum of 65%.
- The ground floor shall be similar for residential uses. Larger openings can be provided for other uses.

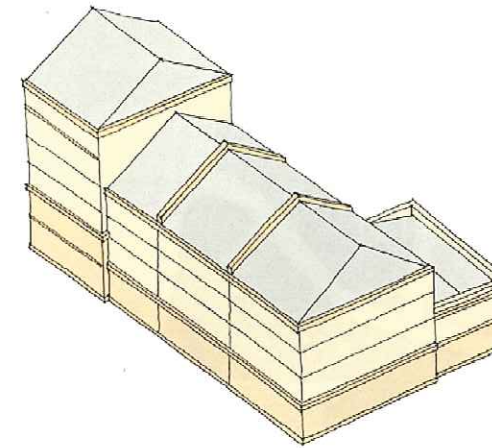
Wharf Building (4–6 Stories)



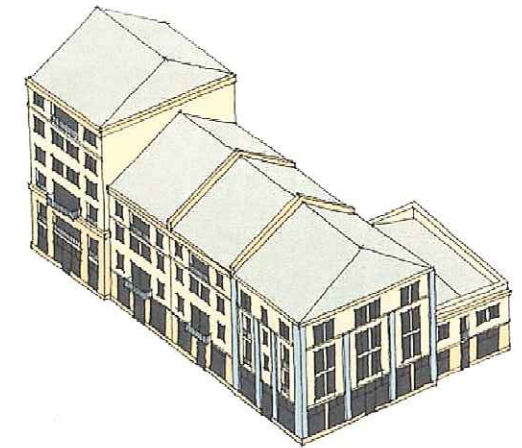
Building zone



Building envelope



Building articulation



- The massing of buildings facing this address should be rectangular and sited perpendicular to Pretty Lake Avenue.
- The primary facade of buildings shall front the view corridor easements as determined with the building regulation plan.
- Buildings may be 4 to 6 stories tall depending upon their placement within the building regulations plan.
- Sloped roof forms with innovative dormer and roof decks are encouraged.

- The buildings should be articulated to resemble a pattern of individual buildings. This can be accomplished with physical breaks in the facade, by recessing small lengths of the facade (minimum 3'0" by 2'-0" deep), by changing the plane of the facade (no more than 2'-0") or by projecting bearing walls between bays of the buildings.
- These changes in plane should occur in intervals of no less than 20 feet and no more than 40 feet. The result will be a collection of individual facades.
- The ground floor should be expressed as a 'base.'

- Canopies, bay windows, balconies, and roof elements are encouraged. Each individual facade should have a minimum of one such element.
- Canopies and other extensions into the front yard space may not exceed 35% of the lineal frontage of the facade.
- Windows should be of vertical, classically-proportioned openings but with more window than solid.
- Windows shall occupy a minimum of 50% of the lineal frontage of each facade and a maximum of 80%.
- Where there are continuous bands of windows, they must be articulated as series of individual, vertically-proportioned windows.
- The ground floor shall be similar for residential or office uses. Store front type windows should be used for retail uses on the ground floor.

District Character Areas

THE BASIS OF THE East Beach Harbor Mixed-Use Waterfront District is its context and adjacency to the neighborhood of East Beach and Little Creek Channel. The diverse and unique qualities of its neighbors extend into the District itself so that the area is grounded in the context of the city.

Three distinct character areas are proposed for the East Beach Mixed-Use Waterfront District: The East Beach Character Area, the Pretty Lake Avenue Character Area and the Harbor Character Area.

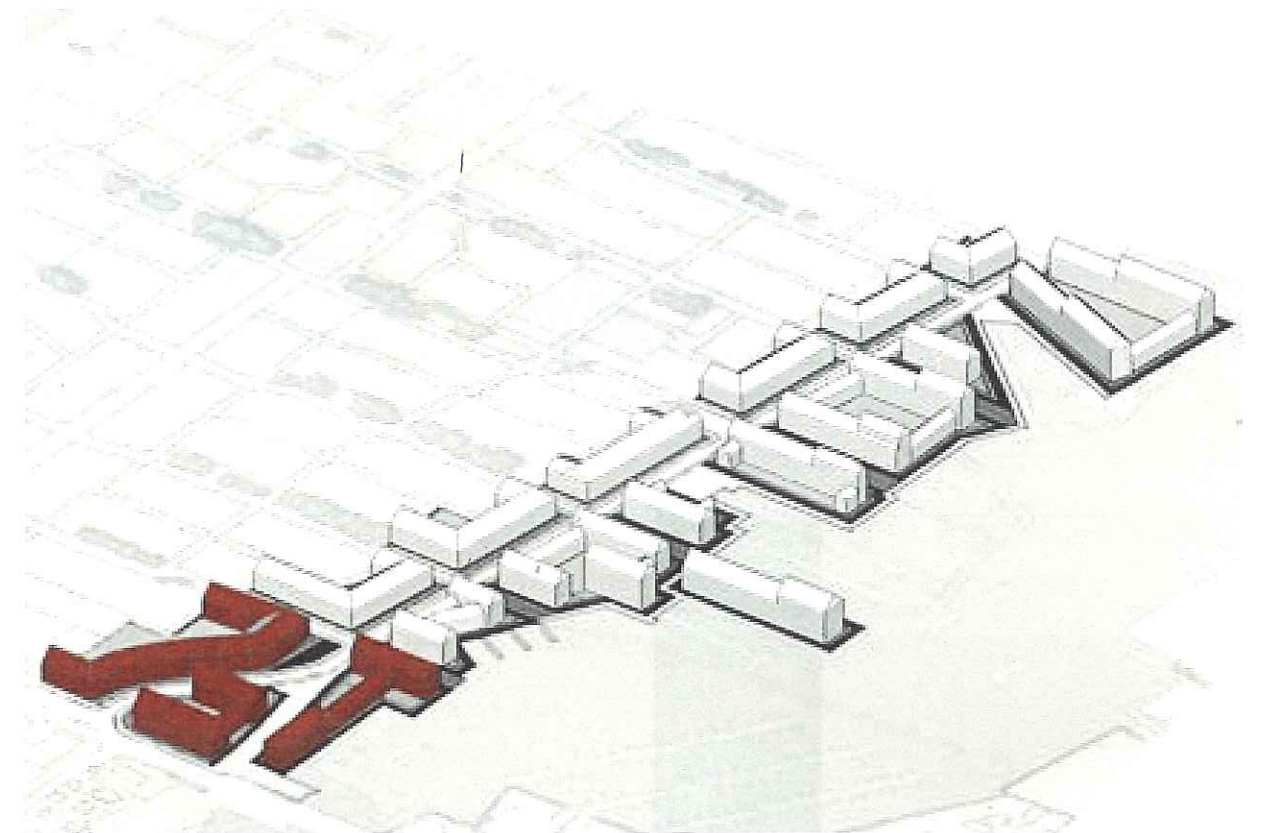


East Beach Character Area

The East Beach Character Area is envisioned to be a vibrant mixed use district connected to the image of sailing and the water. Three to four story buildings with residential units over retail, office, and restaurants line Pretty Lake Avenue and provide a lively public experience. Until recently, most blocks in Norfolk and other cities have been built in small increments over time, creating a richness of street wall character that comes with many contributions to the creation of the city. Current development is often built in larger, more monolithic increments. These newer blocks have lost the richness and texture of the older city and are often unrelenting, horizontal, and boring. These guidelines illustrate how large buildings can be articulated into smaller more humanly scaled increments.



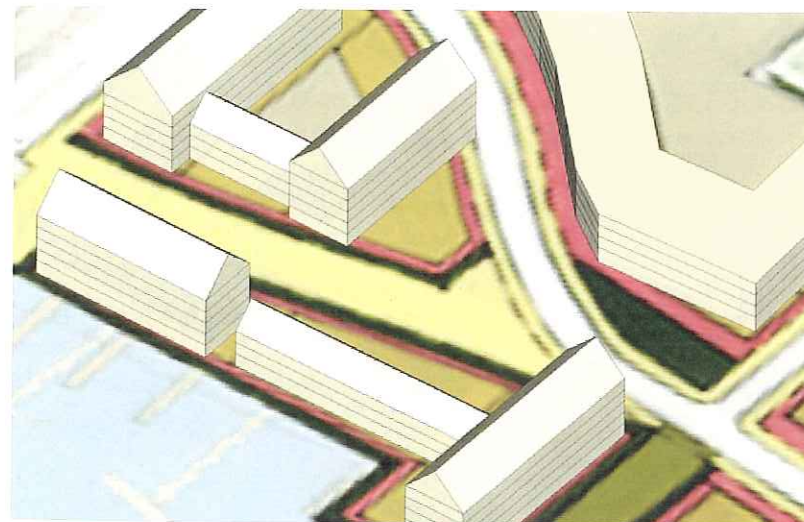
Character sketch of the East Beach Area from the East Beach Pattern Book.



EAST BEACH CHARACTER AREA



Building zone



Building envelope



Building articulation

Option A

Building Location

- All buildings must be located within the building zone.
- Continuous building frontage in each block is required along Pretty Lake Drive and Shore Drive.
- No space is permitted between separate buildings along Pretty Lake Drive.

Front Yard

- A zero front yard is required along Shore Drive, Pretty Lake Drive, and 24th Bay Street unless otherwise noted within the General Development Standards.

Facade Zone

- Balconies and stoops are permitted to project within the facade zone, but may not project beyond the property line.

Building Setbacks

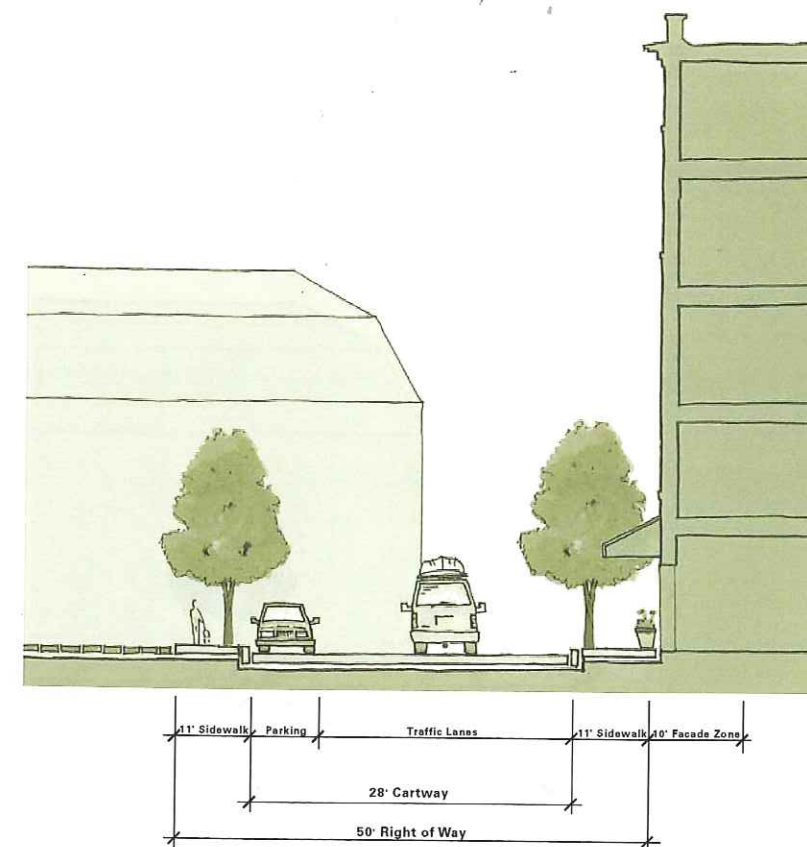
- All buildings will have a zero setback from the property line unless otherwise noted in within the General Development Standards.

Building Height

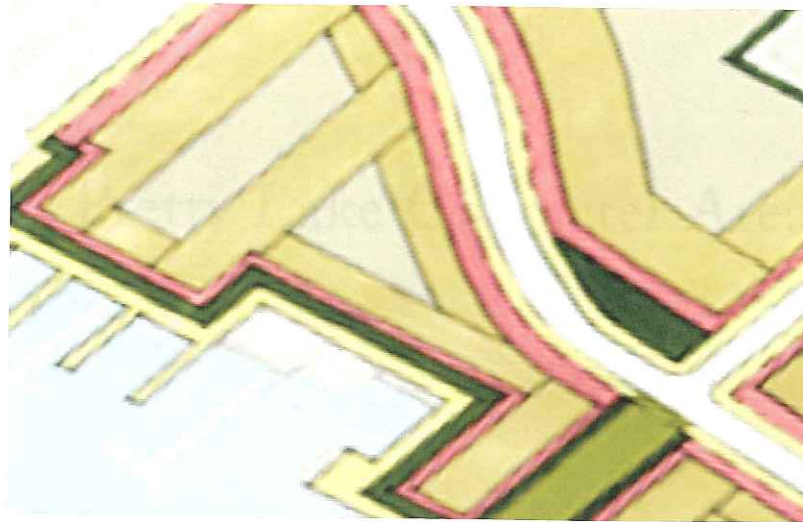
- The buildings on Pretty Lake Drive and Shore Drive will be 4 stories in height (maximum 45.00 feet).
- Buildings on 24th Bay Street will be 3 stories in height maximum.

Building Access

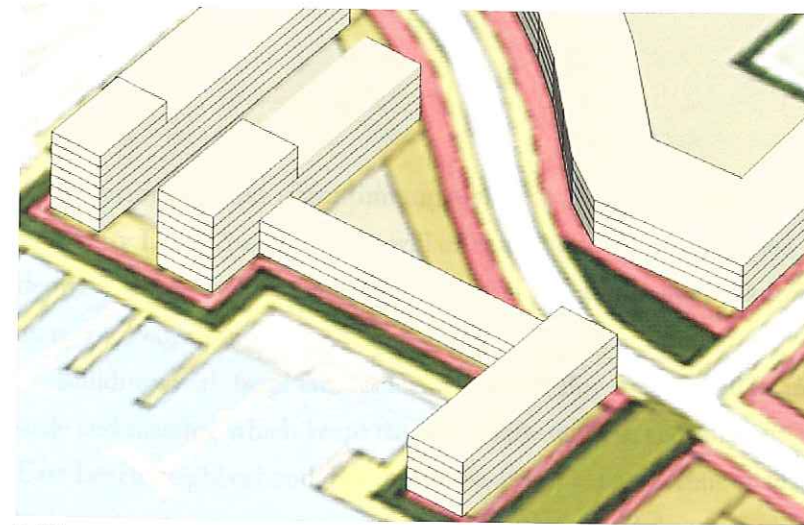
- The primary entrance to ground floor non-residential uses shall face Pretty Lake Drive, Shore Drive, or 24th Bay Street.
- The primary entrance to residential units shall face Pretty Lake Drive, Shore Drive, or 24th Bay Street.
- No entry drives are permitted along Pretty Lake Drive. A vehicle drop-off is permitted in the courtyard behind.
- Parking garages and service areas cannot be accessed from the Pretty Lake Drive facades. Access to these areas is permitted from side street facades, side yards, and courtyards. All dumpster and loading docks must be screened from the streets with garage doors, screen walls, plantings, or other devices.
- A maximum of one open-air, pedestrian passageway penetration to a courtyard is permitted for each block.



Section through Pretty Lake Drive at "East Beach Character Area"



Building zone



Building envelope



Building articulation

Preferred Option

Building Location

- All buildings must be located within the building zone.
- Continuous building frontage in each block is required along Pretty Lake Drive and Shore Drive.
- No space is permitted between separate buildings along Pretty Lake Drive.

Front Yard

- A zero front yard is required along Shore Drive, Pretty Lake Drive, and 24th Bay Street unless otherwise noted within the General Development Standards.

Facade Zone

- Balconies and stoops are permitted to project within the facade zone, but may not project beyond the property line.

Building Setbacks

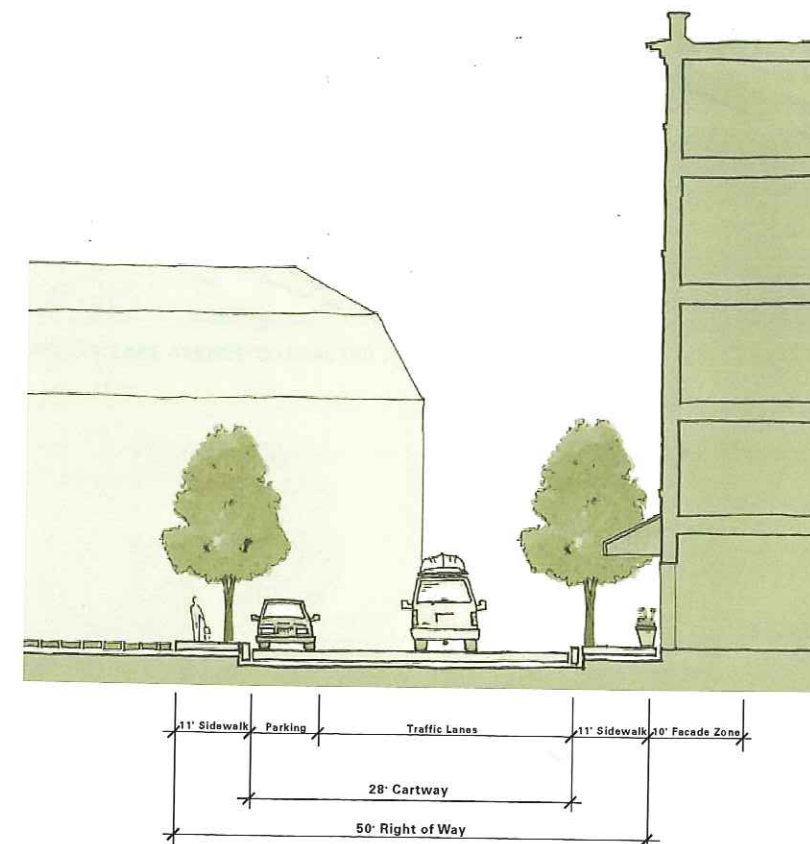
- All buildings will have a zero setback from the property line unless otherwise noted in within the General Development Standards.

Building Height

- The buildings on Pretty Lake Drive and Shore Drive will be 4 stories in height (maximum 45.00 feet).
- Buildings on 24th Bay Street will be 3 stories in height maximum.

Building Access

- The primary entrance to ground floor non-residential uses shall face Pretty Lake Drive, Shore Drive, or 24th Bay Street.
- The primary entrance to residential units shall face Pretty Lake Drive, Shore Drive, or 24th Bay Street.
- No entry drives are permitted along Pretty Lake Drive. A vehicle drop-off is permitted in the courtyard behind.
- Parking garages and service areas cannot be accessed from the Pretty Lake Drive facades. Access to these areas is permitted from side street facades, side yards, and courtyards. All dumpster and loading docks must be screened from the streets with garage doors, screen walls, plantings, or other devices.
- A maximum of one open-air, pedestrian passageway penetration to a courtyard is permitted for each block.

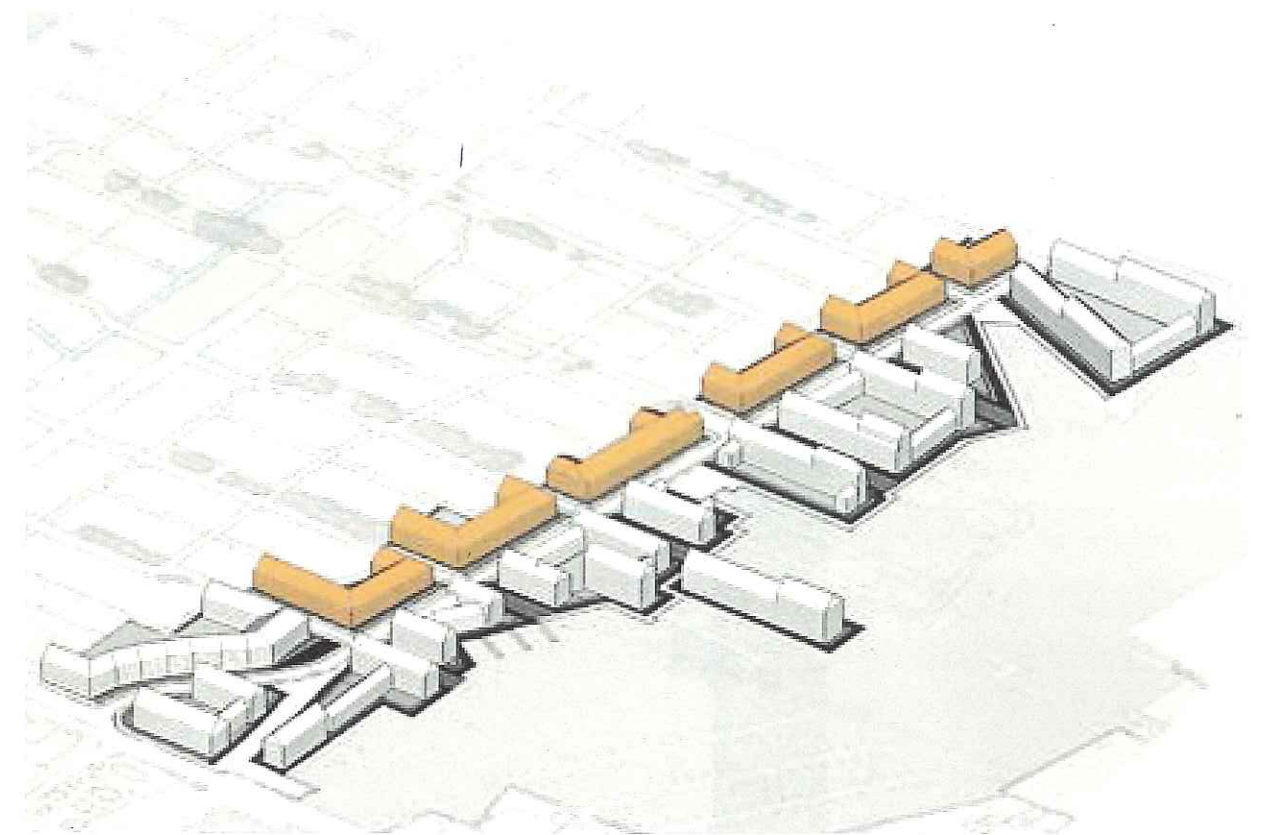


Section through Pretty Lake Drive at "East Beach Character Area"

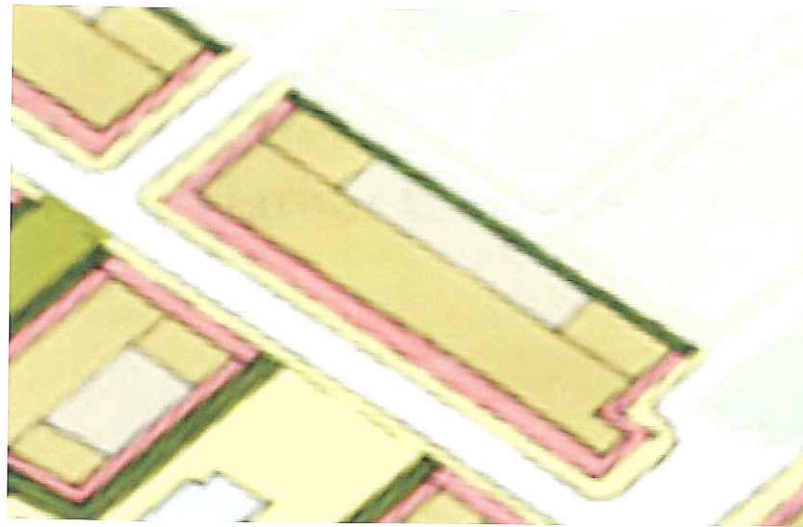
Pretty Lake Character Area

As the primary public street running east-west along the Channel, the Pretty Lake Character Area will offer a mixture of attached residential buildings which provide an informal yet continuous backdrop along the waterfront.

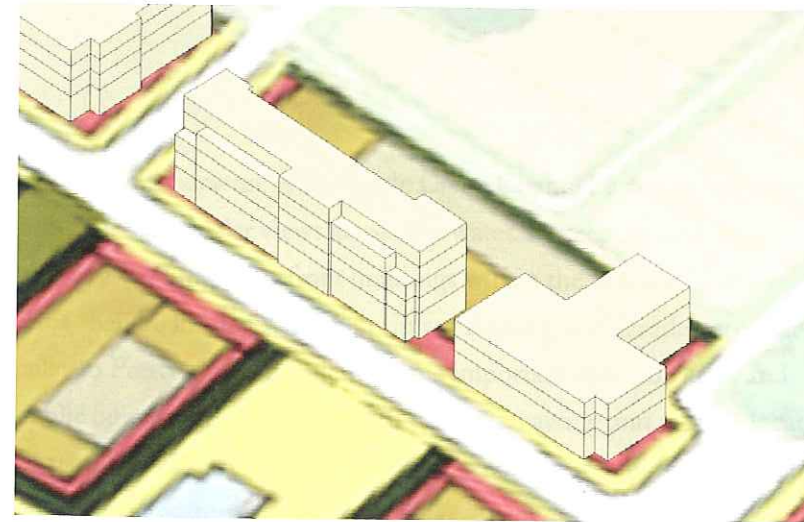
Buildings with front facades located on the Bay streets shall be in scale and massing which keeps them in character with the adjacent East Beach neighborhood. Facades along Pretty Lake Avenue shall be in scale and massing as to provide a continuous and drastic background from the Channel.



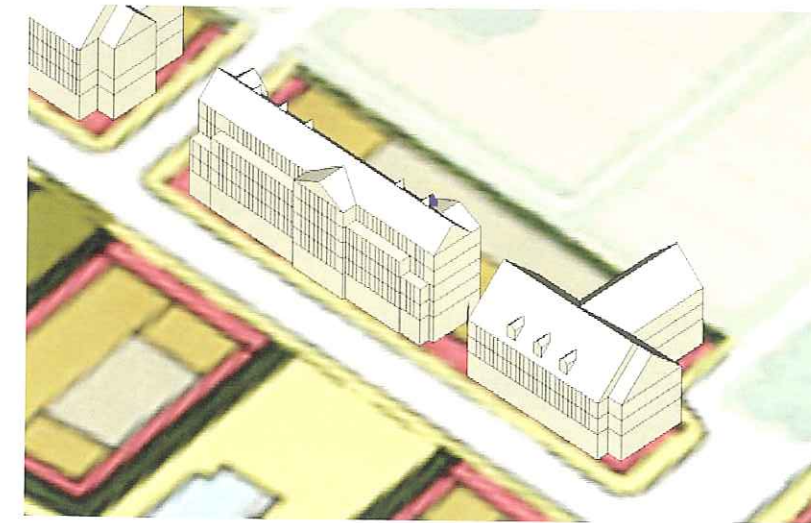
PRETTY LAKE AVENUE CHARACTER AREA



Building zone



Building envelope



Building articulation

Building Location

- All buildings must be located within the building zone.
- Continuous building frontage in each block is required along Pretty Lake Avenue and Shore Drive.
- No space is permitted between separate buildings along Pretty Lake Drive.

Front Yard

- A zero front yard is required along Pretty Lake Avenue, unless otherwise noted within the General Development Standards.

Facade Zone

- Balconies and stoops are permitted to project within the facade zone, but may not project beyond the property line.

Building Setbacks

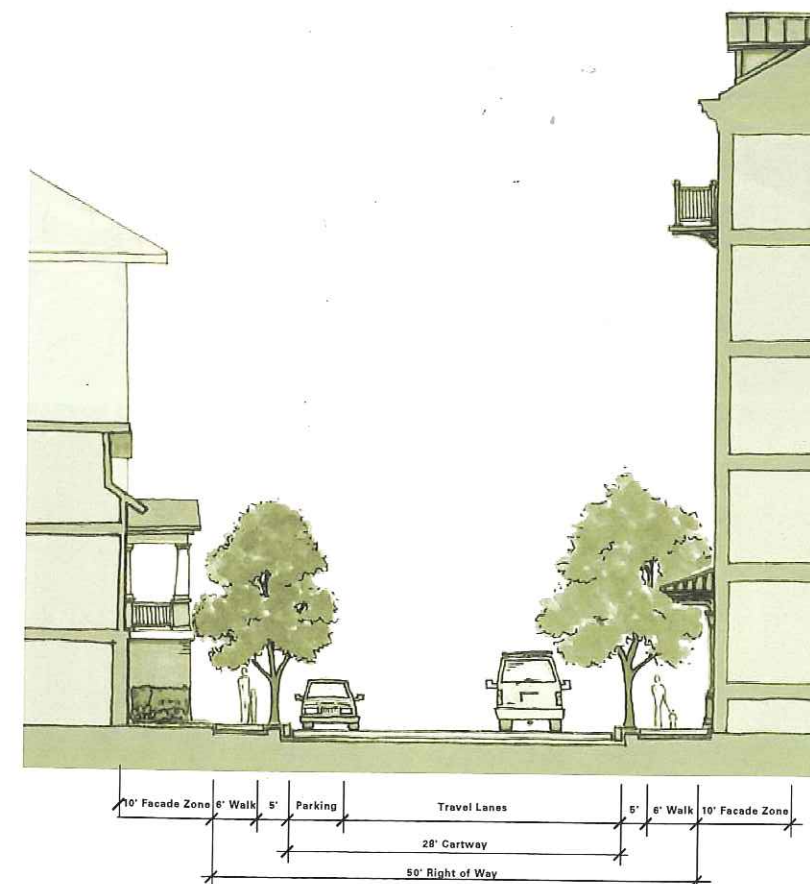
- All buildings will have a zero setback from the property line unless otherwise noted in within the General Development Standards.

Building Height

- The buildings on Pretty Lake Avenue and will be 4 stories in height (maximum 45.00 feet).
- Buildings on Bay Streets will be 3 stories in height maximum.

Building Access

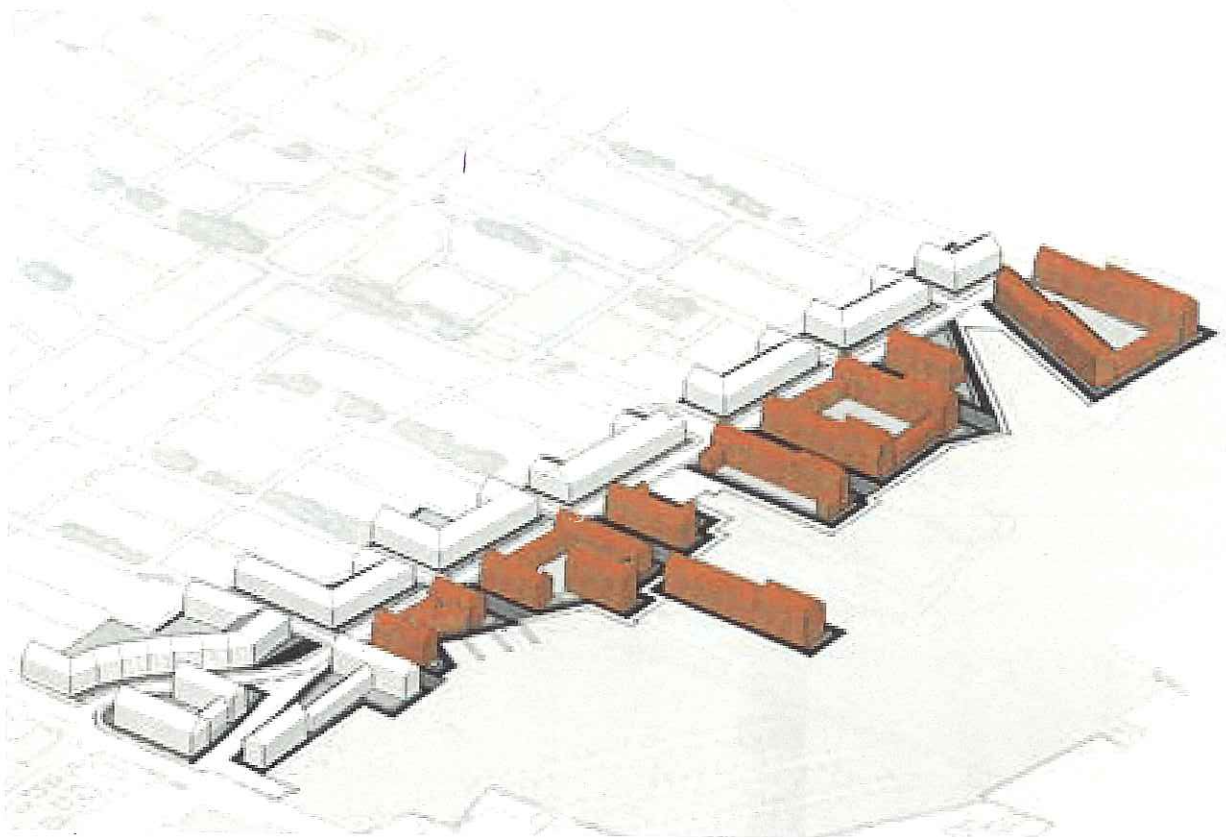
- The primary entrance to residential units shall face Pretty Lake Avenue, or Bay Street.
- No entry drives are permitted along Pretty Lake Avenue. A vehicle drop-off is permitted in the courtyard behind.
- Parking garages and service areas cannot be accessed from the Pretty Lake Avenue facades. Access to these areas is permitted from side street facades, side yards, and courtyards. All dumpster and loading docks must be screened from the streets with garage doors, screen walls, plantings, or other devices.
- A maximum of one open-air, pedestrian passageway penetration to a courtyard is permitted for each block.



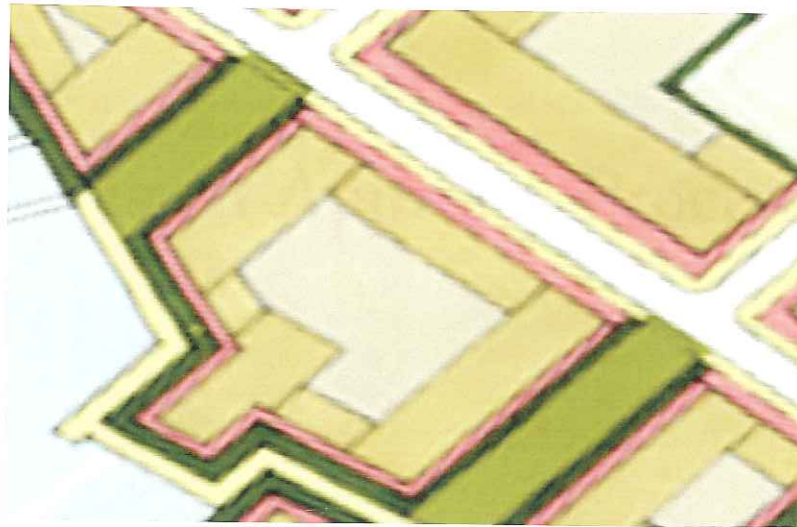
Section through Pretty Lake Avenue

Harbor Character Area

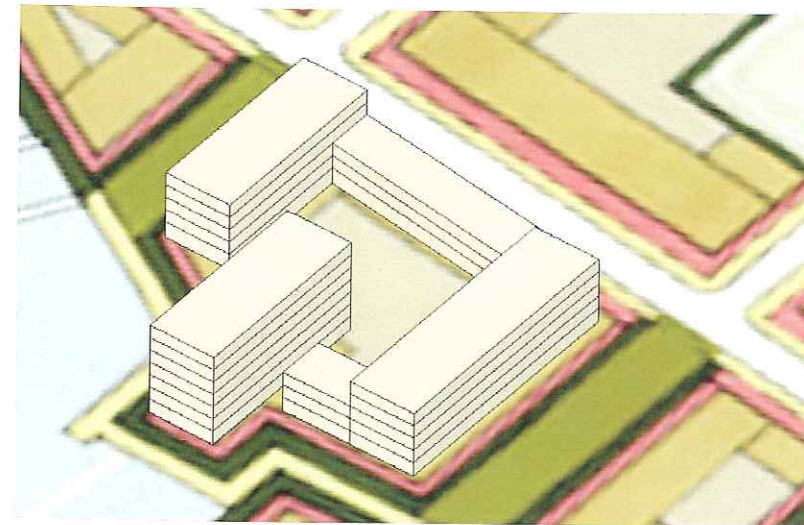
The unique character of the Little Creek Channel provides opportunities for spectacular views across the water. In keeping with its history as a working waterfront buildings within the Harbor Character Area shall be located with the primary massing and facade perpendicular to Pretty Lake Avenue to frame important view corridors and public open spaces. Building with primary massing and facades located parallel to Pretty Lake Avenue shall be lower in building height to preserve views from across Pretty Lake Avenue.



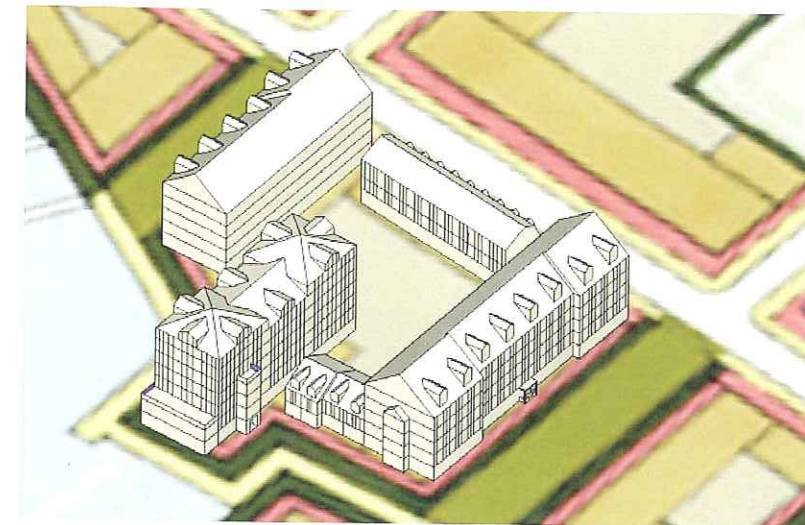
HARBOR CHARACTER AREA



Building zone



Building envelope



Building articulation

Building Location

- All buildings must be located within the building zone.
- Primary building frontage in each block is required along View Corridor Easements.
- No space is permitted between separate buildings along Pretty Lake Avenue.

Front Yard

- A 5-foot front yard is required along View Corridor Easements
- A 10' front yard is required along waterfront pedestrian sidewalk easements.

Facade Zone

- Balconies and stoops are permitted to project within the facade zone, but may not project beyond the property line.

Building Setbacks

- All buildings will have a zero setback from the property line unless otherwise noted in within the General Development Standards.

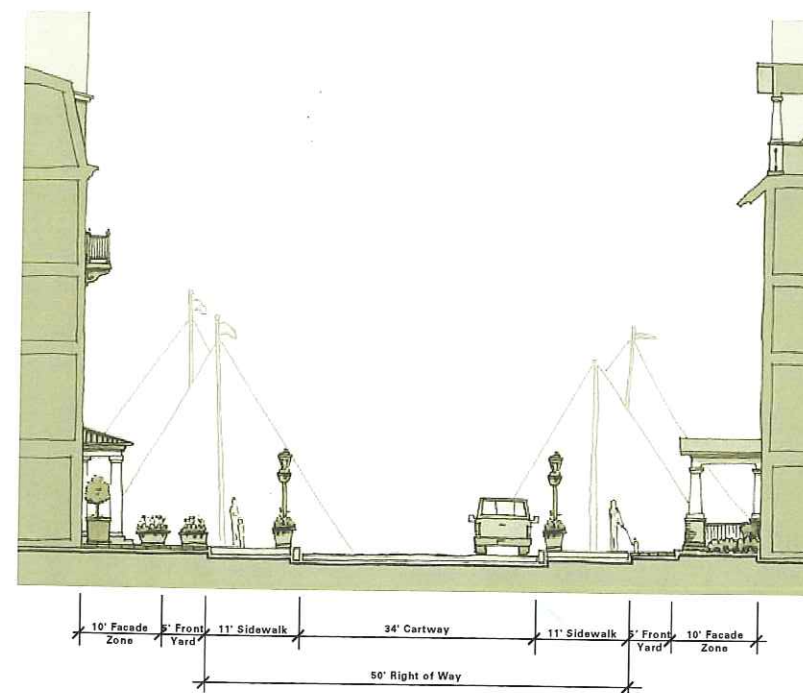
Building Height

- The buildings on Pretty Lake Avenue and will be 2 stories in height (maximum __ feet).
- The buildings on View Corridor Easements and will be 4-6 stories in height (maximum __ feet).
- The buildings on waterfront pedestrian sidewalk easements will be 2 stories in height (maximum __ feet).

Building Access

- The primary entrance to residential units shall face View Corridor easements, waterfront pedestrian easements or Pretty Lake Drive.
- A maximum of one entry drive is permitted along Pretty Lake Drive. A vehicle drop-off is permitted in the courtyard behind.
- A maximum of one entry drive is permitted along View Corridor Easements. A vehicle drop-off is permitted in the courtyard behind.
- Access to parking garages is permitted from side street facades, side yards, and courtyards. All dumpster and loading docks must

- be screened from the streets with garage doors, screen walls, plantings, or other devices.
- A maximum of one open-air, pedestrian passageway penetration to a courtyard is permitted for each block.



Section through View Corridor Easement

DRAFT

Design Approval Process

© 2005 URBAN DESIGN ASSOCIATES